

Portsmouth's Transport Recovery Plan – Draft

JUNE 2020

Transport recovery plan

Connecting active travel and onward journeys

This map shows the proposed Emergency Active Travel Fund (tranche 1) and Reopening Highstreets Safely Fund measures alongside existing cycling routes. Further improvements to existing routes and the addition of new infrastructure will be brought forward from the Local Cycling and Walking Infrastructure Plan and if bidding for tranche 2 funding is successful.



Temporary improvement measures

	Safe walking routes
	Park and Cycle
	Additional pedestrian space
	Passing place
	Pedestrian crossing
	Cycling and walking space
	Pop up cycle lane
	Low traffic neighbourhood
	No through route for vehicles
	Bus stops

Existing quieter cycle routes*

	Paulsgrove ↔ The Hard <i>Connects to purple route at Paulsgrove</i>
	Cosham Station ↔ Fratton Station
	Havant Road ↔ Milton Parade and Esplanade
	Commercial Rd/Guildhall ↔ Seafront
	Fratton Park ↔ The Pyramids Centre
	Paulsgrove ↔ Eastern Road <i>Connects to green route at Paulsgrove and orange route on Eastern Road</i>
	Northern Parade ↔ Eastern Road <i>Connects to green route at Northern Parade and orange route on Eastern Road</i>
	Rudmore Roundabout ↔ Eastern Road <i>Connects to green route at Rudmore roundabout and orange route on Eastern Road</i>
	Portsmouth & Southsea Station ↔ Milton Locks
	Gunwharf Quays ↔ Hayling Island Ferry/Eastney
	Cycle infrastructure*

*This identifies areas that have items in place for cycling including cycle lanes and shared paths. Note that some of the quieter routes also have areas with cycle infrastructure in place too.

Contents

Transport recovery plan	2
1.0 Managing the impact of COVID-19	4
2.0 Current Context	5
3.0 Supporting a resilient city	6
4.0 Draft Local Transport Plan 4	8
5.0 Proposed Activities	10
5.1 Delivering cleaner air	10
5.2 Prioritising walking and cycling	11
5.3 Transforming public transport	12
5.4 Supporting business and protecting our assets	14
6.0 Communication and Engagement	16
7.0 Next Steps	18
Glossary of terms	20

1.0 Managing the impact of COVID-19

The Coronavirus Pandemic necessitated immediate and wide-spread changes to the way the city operates and to each of our day to day lives. To slow the spread of the virus a nationwide 'lockdown' was imposed that dramatically reduced travel demand as offices, shops, industry and schools closed. We expect that there will continue to be significant disruption to travel over the coming year, not least because until a vaccine is found social distancing restrictions are likely to remain in place meaning the capacity of public transport will be dramatically reduced. If everyone who previously

caught the bus or train chose instead to drive there will be gridlock and air quality would worsen.

However, even out of this terrible crisis there are opportunities; individuals and businesses have learnt how to make best use of digital meeting platforms and home working which has reduced commuter travel, there has been an increased uptake in walking and cycling as people made this part of their daily exercise, reduced traffic levels have resulted in less noise, air pollution and safer streets, while many have had the opportunity to explore their local area increasing their appreciation of the local community they live in.



2.0 Current Context

Following the announcement from the Government in mid-May regarding the gradual easing of the lockdown restrictions implemented in response to the COVID-19 Pandemic, Local Transport Authorities were advised to commence planning to ensure that local transport networks were prepared for the change in travel demand.

Since the government introduced lockdown measures, use of motorised traffic in the city has decreased to as low as 34% of pre-lockdown levels and cycling numbers have increased to as much as 156% compared to last year. However, as restrictions on movement are loosened traffic volumes are continuing to rise with 87% of pre-lockdown traffic now back on the main roads around the city.

The Department for Transport has outlined a number of areas which need to be considered in

preparation for the local transport restart, and in particular in response to the green and active travel agenda to 'help embed altered behaviours and demonstrate the positive effects of active travel' as requested by Transport Minister Grant Shapps:

- Collaboration
- Green Transport Restart and Recovery
- E-Scooters
- Data and Evidence
- Public Transport
- Travel Planning
- Role of the Car
- Transport Users
- Resources



3.0 Supporting a resilient city

The transport network is identified as being central to local economic recovery and it is expected that resources are made available and quickly mobilised to ensure that the network is as prepared as possible for when the government changes, and in time, removes all travel restrictions.

Portsmouth City Council is working closely with our liaison officer at the Department for Transport (DfT) to share progress and seek assistance in any areas of concern.

Portsmouth must be resilient in the face of the COVID-19 crisis. The transport response will focus on keeping people safe and the city moving.

The following principles are essential and will guide this process:

Reduce travel demand. To cope with reduced capacity on public transport while ensuring the highway network continues to function safely. We will encourage businesses and organisations to reduce travel demand during peak hours. This will include encouraging greater working from home, staggering arrival and departure times for staff and pupils and re-moding journeys to non-motorised modes where possible. We will facilitate greater use of on-line services by providing improved consolidation facilities, including local collection lockers that can minimise the impact of multiple delivery vehicles.

Making best use of limited capacity. Due to reduced capacity of public transport and not enough space for everyone to drive on the highway network. For example one traffic lane

can carry 2,000 people per hour in cars, 14,000 on bikes or 19,000 on foot[1]. To ensure that everyone who needs to travel can do so quickly and safely we will prioritise walking, cycling and emerging modes such as e-scooters above car use; something that is all the more challenging in Portsmouth's narrow streets. We will focus our efforts on key routes to the city centre, local centres, health and education facilities and major employment sites, using research gathered through LCWIP route audits and consultation with ward councillors.

Keeping people safe. If people are to have the confidence to access the city over the coming months. There will be places where people continue to gather during this crisis, including bus stops and train stations, shops and pharmacies. We will provide more space for pedestrians where possible in these locations so that they can access the city safely, taking into account social distancing guidelines.

Improving our lives locally. There is likely to be a continued increase in local travel within our neighbourhoods as work from home becomes a norm and residents carry out their day to day activities nearby. To give people the confidence to walk and cycle locally we will provide additional space for walking and cycling and reduce 'rat running' traffic in residential streets and provide improved connections to local centres and green spaces through initiatives such as low traffic neighbourhoods and school streets.

In prioritising, designing and delivering schemes and supporting packages within this plan, we will:

- follow the draft vision and objectives of the LTP 4, agreed by Council on the 10th March 2020;
- maximise opportunities provided by existing and planned projects and funding.
- identify opportunities to align delivery with the planned highway maintenance programme;
- implement schemes as trials or temporary measures (using Temporary and/or Experimental Traffic Regulation Orders initially), with options for future conversion to permanent status where appropriate;
- co-ordinate with other highway and planning processes;
- engage and work with the Ward Councillors, Solent Transport, partners, and key stakeholders including business and transport operators, and ensure that members of the public are kept informed of and involved with any changes being made ;and
- undertake ongoing monitoring and review of all schemes and measures on a regular basis.



4.0 Draft Local Transport Plan 4

The draft Local Transport Plan (LTP 4) vision and objectives were considered and approved by the Portsmouth City Council Cabinet, 10th March 2020. These will guide the measures implemented to restart the local transport provision:

Vision:

'By 2036 Portsmouth will have a people centred travel network, prioritising inclusive, active and sustainable connectivity, that creates a safer, healthier and more prosperous city.'

This vision is supported by four strategic objectives, which in turn are supported by a number of policies which will be developed following the confirmation of the proposed direction of the strategy:

Strategic objectives:



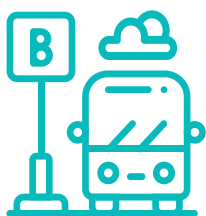
Delivering cleaner air

- Implement a Government directed city centre clean air zone in 2021
- Supporting infrastructure for alternative fuelled vehicles
- Maintaining residents' parking permit system alongside policy that encourages fewer and greener vehicles
- Expand the Portsmouth park and ride to reduce pollution and congestion in the city centre
- Explore private non-residential parking restrictions to encourage mode shift and help pay for improved walking, cycling and public transport infrastructure



Prioritising walking and cycling

- Reallocate road space to walking and cycling to deliver:
 - › A network of attractive, inclusive and accessible walking routes and associated pedestrian priority
 - › A comprehensive network of continuous cycle routes
 - › Cycle parking in local centres and street
- Reduce through traffic in residential neighbourhoods, and manage parking through parking controls
- Improve the city centre, local and district centres by reducing or removing general traffic, with access focussed on sustainable methods
- Deliver innovations in micro mobility to promote transport choice and active transport options



Transforming public transport

- Develop a rapid transit network that connects key locations in the city with South East Hampshire, and facilitates the future growth of Portsmouth
- Prioritise local bus services over general traffic to make journeys by public transport quicker and more reliable
- Work with transport operators to deliver integrated, efficient and affordable services promoting local and regional connectivity
- Work with bus operators to explore alternative fuels for the bus network within Portsmouth
- Deliver physical and digital integration of travel modes and fares to improve the usability of the travel network
- Deliver high quality public transport interchanges, stations and stops as well as first mile/last mile connectivity to them



Supporting business and protecting our assets

- Protect access to the port and naval bases
- Support businesses and other organisations to consolidate their operational journeys, including use of zero emission vehicles for last mile deliveries
- Deliver a micro and macro freight consolidation centre to serve Portsmouth's businesses, and support opportunities for increased rail freight
- Develop and deliver schemes to incentivise behaviour change and minimise highway occupation at peak times in sensitive locations such as applying charges through lane rental
- Ensuring a high quality, well-functioning highway network through developing and delivering maintenance of all highways and transport assets
- Management of the kerbside to deliver variable use for essential access

5.0 Proposed Activities

The following activities are proposed in relation to delivering the 4 key objectives of the LTP 4 as part of our transport response to COVID-19.

5.1 Delivering cleaner air

Supporting infrastructure for alternative fuelled vehicles

We are continuing to roll out additional electric vehicle charging points in the city and we are recruiting a dedicated project manager to accelerate the delivery of this essential infrastructure. As more of our residents return to work we will be encouraging them to travel to their place of work using active modes where possible. However, we are aware that this will not be an option for everyone and therefore for those who few who have to use a car it is essential that we enable as many of these car journeys as possible to be made in the least polluting vehicles.

We are also working closely with our partners Solent Transport and with the DfT to bring forward a trial of hired electric scooters in the city. We have submitted an expression of interest to the department for Transport for Portsmouth to be included in the trial, and we expect trials of e-scooters to be launched in the city by 31st August. It is hoped that such a trial could provide a realistic alternative to some car journeys in the city and we are talking to businesses about how the trial could help with their return to work.

Maintaining residents' parking permit system

Whilst government guidance to England was to stay at home, we temporarily suspended the

operation of residents parking zones and made council car parks and on-street pay and display parking free, to support key workers during a time when few vehicles were moving. The focus moved to safety critical parking enforcement. As government eases lockdown restrictions we expect vehicle movement to continue increasing and it is essential we support residents by responding to this change and manage parking through lifting the suspension on residents parking zones. From 1 June the enforcement of residents parking zones and pay and display car parks resumed.

Portsmouth Park & Cycle to reduce pollution and congestion in the city centre

The Transport Secretary encouraged people to drive close to, but not right into a town or city centre where they work. The idea being they could park up and then walk or cycle for the remainder of the journey.

Park and Cycle provision is being considered from the Portsmouth Park and Ride, Junction 1 of the M275, and from Farlington Playing Fields car park in the East of the city. This will enable commuters to drive the majority of their journey to work, but make the final part of their journey on bike, on an e-scooter or on foot, therefore reducing the pressure on the road network in the centre of the city.

5.2 Prioritising walking and cycling

Reallocation of Road Space

As a majority island city, with only three roads connecting to the mainland, and some of the highest population densities outside London, competition for road space is intense.

In order to support increased numbers of people cycling and walking, and enable social distancing a number of proposals have been put forward, and enacted, to reallocate road space to walkers and cyclists:

- Seafront roads closed
- City centre roads closed:
 - › Charlotte Street - from Landport View to Cascades Approach;
 - › Isambard Brunel Road - from Station Street roundabout to Greetham Street roundabout
 - › Guildhall Walk - from the junction of Alec Rose Lane to the junction of Spring Gardens.

- Suspension of parking to allow for pop-up cycle lanes:
 - › Ordinance Row
 - › The Hard
 - › Elm Grove

We have also submitted an expression of interest to the first round of government's emergency active travel fund, requesting funding to deliver the following measures in addition to those listed above:

- Low traffic neighbourhood between Highland Road and Goldsmith Avenue
- Filtered permeability on Castle Road
- Filtered permeability on Canal Walk

It is clear that the demand for space in the city will change as more people resume normal activity. We are therefore continuing to work with interest groups such as the Portsmouth Cycle Form, Ward Councillors and Portsmouth Friends of the Earth to identify further opportunities for road closures through introducing point closures, play streets or school streets and to respond in an agile way and the focus on transport and mobility shifts.



5.0 Proposed Activities

5.3 Transforming public transport

Bus Service Levels

Bus service levels have been reduced since late March 2020 following the outbreak of COVID-19 and government guidance on travel and at present there are around 20% of the passengers than at this time last year. From 31 May, these have increased to near pre-COVID levels to cater for increased return to work and retail openings.

Initially funding was provided by the DfT to bus operators providing they did not operate more than 50% of their previous commercial network. Further funding from DfT is being made available to support the increase in service levels.

In response to the gradual easing of the lockdown and the increasing numbers of people using public transport, bus operators are increasing bus services in Portsmouth and government is amending its funding provision accordingly.

Portsmouth City Council will continue to work with the bus operators closely and key employers over the coming months to ensure that the most appropriate service is delivered for the city that responds to demand for public transport as people begin returning to work.

Vehicle Capacity

In response to social distancing, new restrictions are being introduced that limit the number of people who can be carried on buses. This is between 9 and 11 passengers per single deck vehicle (dependent on the actual size of the

vehicle) or 20 passengers on a double deck.

This will mean that once a bus reaches this number of passengers the bus will show the bus destination as FULL due to COVID-19 and passengers will be left behind at bus stops. Notices will be placed on the buses and at bus stops to advise passengers of this.

Updated Mobile Phone app

First bus have updated their mobile app this week. Now, in addition to real time journey information and the ability to buy m-tickets on your phone, you can see how many seats are on the next buses before they arrive so you can adjust your plans if needed.

Social Distancing and Bus Services

With recent new guidance from the government, and the easing of lockdown measures in England, the bus companies have now introduced social distancing measures for our services. These include marking seats out of uses and stopping standing. The Council is supporting these messages through our communication channels and through providing information at bus stops.

The Government announced on the 4th June 2020, that the wearing of face coverings will be a compulsory requirement of using public transport from 15th June 2020. This will apply to any passengers travelling on bus, coach, train, tram, ferry and aircraft, on their whole journey to help reduce the risk of transmission when social distancing is not always possible.

Portsmouth City Council are working closely with operators to ensure consistent messaging to passengers, as well as identifying measures to

support this at the Council's key public transport interchanges. Additional risk assessments are being undertaken by all staff working at interchanges to ensure they have access to face coverings.

Bus Stops and Shelters

The Council is responsible for all bus stops and shelters within Portsmouth, and have undertaken audits at the busiest key transport hubs where increased passenger flow could impact on safe waiting, boarding and alighting public transport. We have introduced messaging about social distancing at bus stops at the busiest stops across the city. This will be complemented by footway markings. We have worked with bus operators to introduce locations in which separate boarding and alighting stops at the busiest locations, to reduce potential contact between passengers. Any changes put in place

are being clearly communicated to users and bus operators, and reviewed regularly to ensure that they are working effectively. We are also working closely with our neighbouring authorities to ensure that all public transport messaging is consistent and clear to users.

Gosport Ferry

Portsmouth City Council, together with Gosport Borough Council and Hampshire County Council have come together to provide a two month funding package which, deferral of some monthly charges, will allow continuity of the ferry service.

Communication with Transport Users

We are continuing to deliver our robust strategy for strong and real time communications with all transport users, particularly those with protected characteristics under the Public-Sector Equality Duty.



5.0 Proposed Activities

5.4 Supporting business and protecting our assets

Proactive Traffic Management

As businesses re-open, often with new operating processes to enable social distancing, there is the potential for significant impact on the road network.

The council will work with key businesses to deliver proactive traffic management to minimise congestion on the network, and support the restart of the local economic.

In particular, at sites such as Gunwharf Quays (now reopened) and the Paulsgrove Household Waste Recycling Centre (HWRC)- opened 11th May, we are undertaking pro-active engagement to support their traffic management.

In the case of the HWRC, a temporary traffic management plan was implemented to create additional queuing space, diverting other through traffic via an alternative local road. Demand for the HWRC was managed by allowing cars with odd numbered registration (last digit) on odd-dated days and evens on evens. Queuing time signs were deployed and 2 hrs before the HWRC closure time, any traffic trying to join the queue at the 2 hr marker was advised that they would not get in and to return in two days' time.

Engagement and Travel Planning

As set out by the DfT's latest advice, we will be undertaking engagement with stakeholders and businesses in order to support their return to work and other usual activity. We will be asking employers to consider ways in which their

workforce can continue to work from home where possible. Where this cannot be achieved we will be asking that employers encourage their staff to walk or cycle to work, and consider ways in which they can reduce the number of vehicle trips to their sites, particularly at peak times.

We are aware that many workplaces and other organisations do not have suitable facilities to support an increased number of people walking and cycling to their sites. We will therefore be making funding available that can be used towards providing new or additional facilities for secure cycle parking and changing facilities.

Schools opening to more pupils

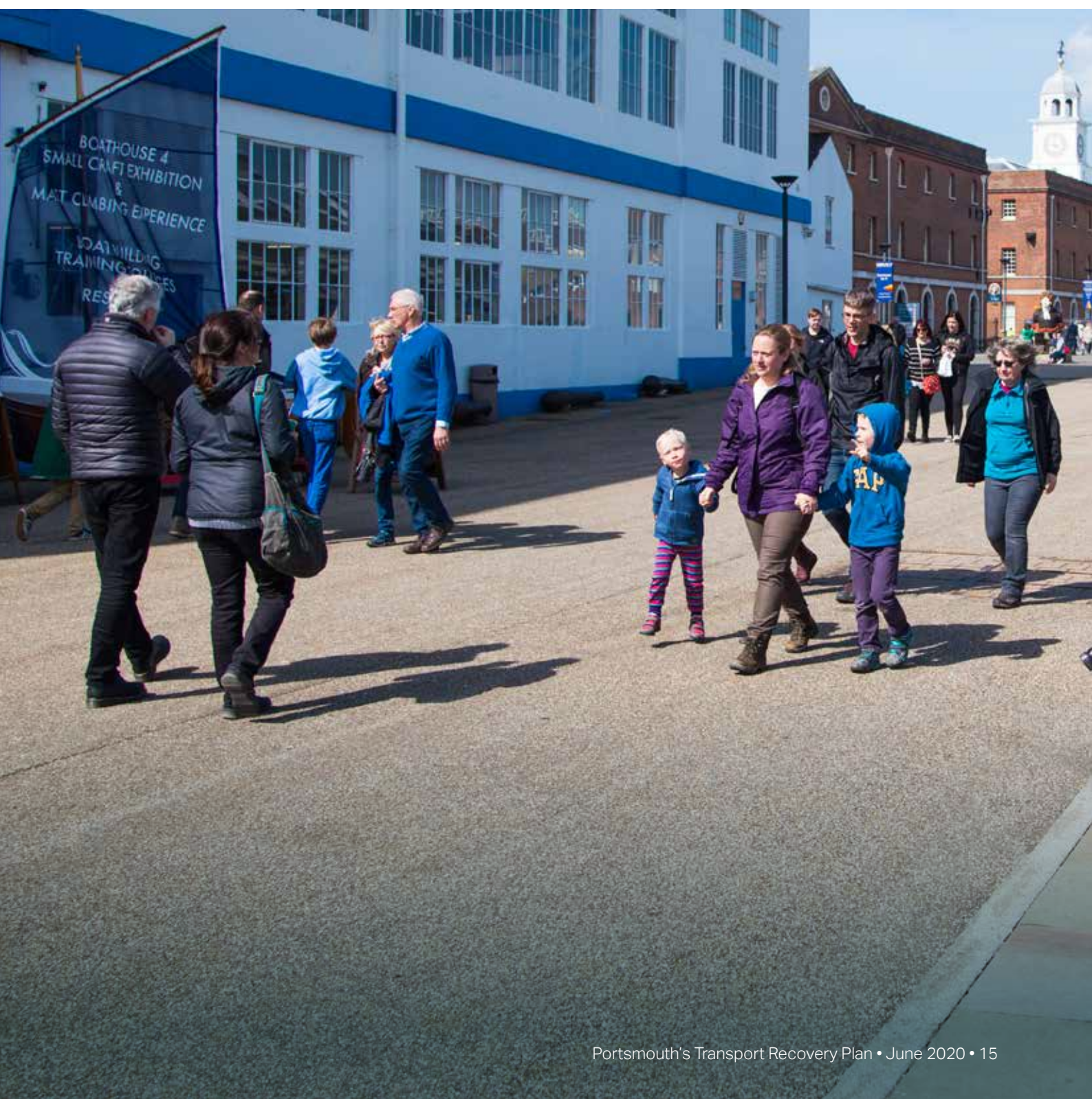
We are engaging with schools to understand their plans for re-opening, and in particular their expectations for how many children will be returning to school and how they will get there. This will help us to work in collaboration with the schools to manage localised congestion hotspots and maintain social distancing on the school run and restarting School Crossing Patrols where possible.

We are continuing to work with colleagues in Education to provide school transport for those children with special educational needs and we are planning for increased demand for this service as schools open to more pupils in September. There is growing pressure on this service as the number of minibuses and staff required to transport the children to school has increased so that social distancing can be maintained. The Transport Operations team are working with colleagues in Education, Human Resources, schools and transport operators to

ensure the continued delivery of the current service and try to plan the for the upcoming school year 2020/21 in very exceptional circumstances.

For Home to School Transport the biggest challenge in September will be the requirement to transport all of the children who have a statutory entitlement in a safe and socially distanced manner.

For more mainstream schools there is an expectation that all pupils will to return to school, we are working with colleagues across the council and with a number of schools in the city to consider how we can implement 'School Streets' and similar measures to improve road safety outside of the school gates.



6.0 Communication and Engagement

Communications and promotion

We have a variety of communication channels to inform residents and businesses about greener, cleaner and safer travel during the COVID-19 recovery and to provide an opportunity to comment on the temporary measures that we introduce. We will also use our communication channels to reinforce government messaging. Some channels are targeted to specific audiences, others provide an opportunity to share messages more widely. Communication channels that we will use to keep the city updated about the emerging plans and individual schemes include social media, the PCC website, Flagship magazine and real time information signs.

Engagement

We are changing the approach to stakeholder engagement for the emergency transport response to the pandemic as required by government to make swift changes. Stakeholders within areas that are being changed will be informed of the changes and have a way of feedback any questions or changes they made need so that their business can operate.

Comments and views will be welcomed once measures are in place and operational, and we will be as responsive as possible to these comments. This ongoing dialogue with stakeholders and communities will help to inform future scheme development and decisions about the continuation or indeed permanence of measures going forward.

We will engage a wider audience through the normal communications methods identified in the previous section.



Consultation Programme

A number of road closures schemes to reallocate road space to walking and cycling are being introduced to promote public safety by enabling social distancing in response to the COVID-19 pandemic.

The legal basis for these schemes is Section 14:2b of the Road Traffic Regulation Act 1984, which gives highway authorities the right to close roads without consultation for up to 42 days (a 21 day closure which can be renewed once) if the public safety need is so great as to warrant this action.

As such, Portsmouth City Council is not ordinarily able to consult and engage with the public and stakeholders in the way that we would ordinarily do.

Comments and views will be welcomed once measures are in place and operational, and we will be as responsive as possible to these comments.

It is, however, of course vitally important that stakeholders, residents and businesses are informed of forthcoming changes. As such, the following processes are in place to give information about the closures, and this will be undertaken with as much advance notice as possible:

- Briefing with Opposition Spokespeople
- Letter outlining changes emailed to all affected Ward Councillors
- Press Release
- PCC Website Updates
- Social Media Campaign (Twitter, Facebook etc.)
- Statutory Stakeholders as per obligations in Section 14.2b
- Letter outlining changes emailed to all affected businesses and residents

- Letter to members of the Transport Liaison Group which has representatives from the following transport organisations:
 - › AA
 - › Aquacars
 - › Blue Lamp Trust
 - › British Cycling
 - › City Wide Taxis
 - › Clearchannel
 - › Cycling UK
 - › Driver and Vehicle Standards Agency
 - › Driving Instructors Association
 - › Driving-Pro.com
 - › Federation of Small Businesses
 - › First Group
 - › Freight Transport Association
 - › Friends of the Earth
 - › Gosport Ferry
 - › Govia Thameslink Railway
 - › Gunwharf Quays
 - › Hampshire Constabulary
 - › Hovertravel
 - › Independent taxi trade representative
 - › Motorcycle Action Group
 - › Portsmouth Cycle Forum
 - › Portsmouth Group Ramblers
 - › Road Haulage Association
 - › South Western Railways
 - › Stagecoach
 - › Sustrans Ranger / Portsmouth Cycle Forum
 - › Uber
 - › University of Portsmouth
 - › Vehicle and Operator Services Agency
 - › Walking Friends Portsmouth
 - › Wightlink

7.0 Next Steps

We will continue to work quickly to deliver temporary changes to the local transport system to enable the restart of the economy in a safe and sustainable way. Many of these changes will need to be made at short-notice to address immediate safety concerns to allow social distancing. However, as we move through the later stages of Government’s roadmap to lifting the Covid-19 restrictions our focus will shift towards the role that local transport can play in maximising the number of activities that can take place safely.

As we adjust to new ways of working, spending our leisure time and moving around the city it will be vital that we take the opportunity to shape the travel habit of residents and visitors to city. We will build upon the principles embedded in our Covid-19 response of: reducing travel demand; making best use for limited capacity; keeping people safe; and improving lives locally to deliver a Local Transport Plan 4 that is transformational for the city.

Action Plan

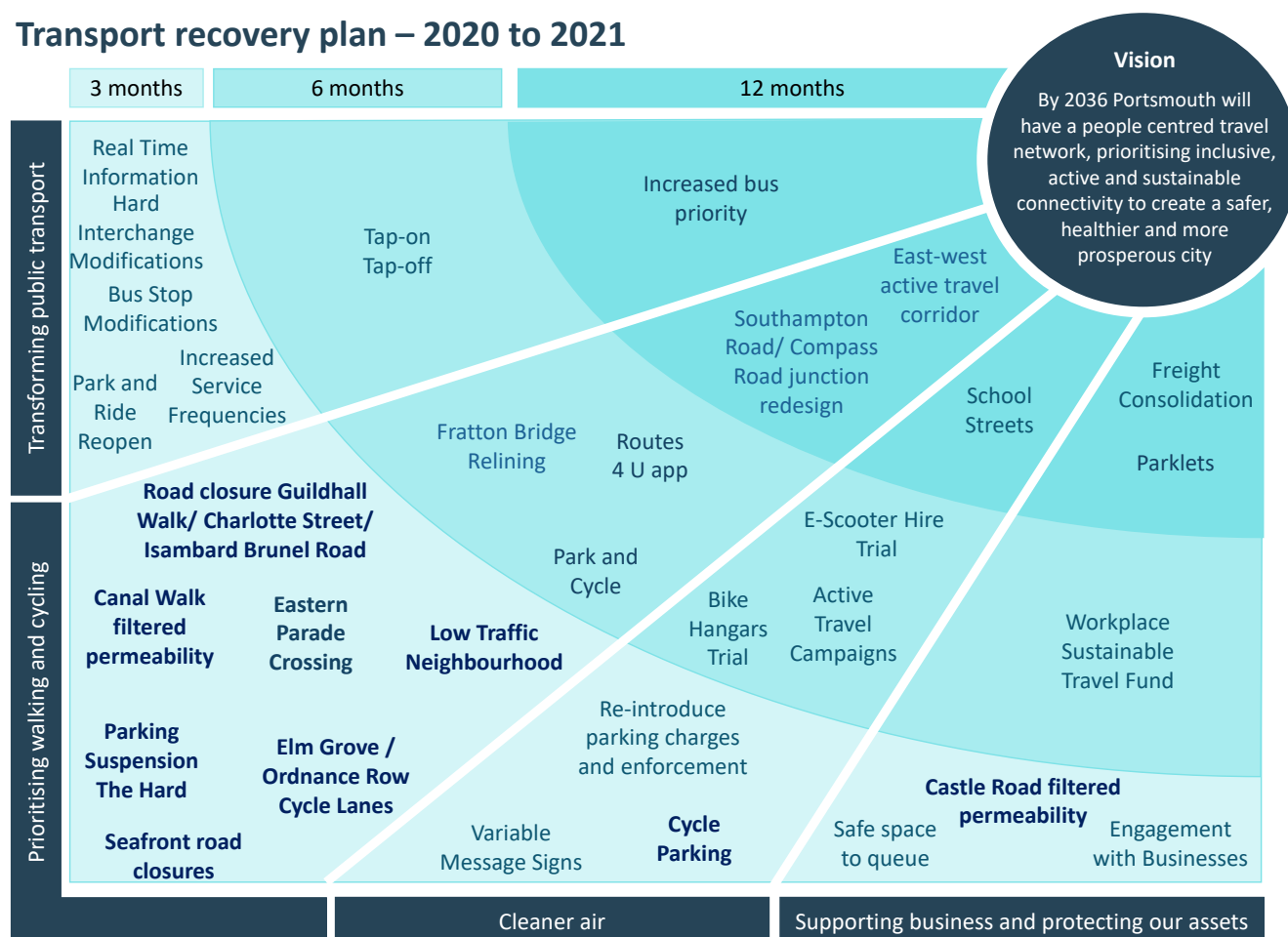
Government have announced two rounds of funding that can be accessed by Local Authorities to deliver the emergency changes to transport needed to support the recovery from Covid-19. The initial round of funding is focused on delivery of measures that can be implemented quickly to provide more space for people to move around the city safely. These measures will be in place in the city by the end of June.

The second round of funding is focused on delivery of longer term projects that can help to make permanent improvements to the local environment by providing a safe and coherent network of active travel infrastructure to support they city’s recovery.

The diagram oppisite sets out the timeline for delivering measures in the short, medium and long term that will be implemented to manage the impact of COVID-19 and help towards achieving the objectives of Portsmouth LTP4.

Short Term	Medium term	Longer Term
These are the activities that we will take within the next 3 months to act in an agile way to provide space for social distancing and ensure that key transport infrastructure is running smoothly	These are the activities that we will undertake in the next 3-6 months to drive the local transport recovery in the city and to help our residents to travel around the city by active and sustainable modes	These measures look beyond the next 6-12 months and towards the delivery of our Local Transport Plan 4

Transport recovery plan – 2020 to 2021



Note: Activities that we are seeking Government emergency active travel funding for are highlighted in bold

Evaluating impact

To ensure the success of the emergency response interventions is understood by PCC we will continue to collect data on both vehicular and cycle traffic within the network.

This data is being used to inform short term emergency interventions, but will also inform how those interventions evolve over time and is being shared with the DfT on a regular basis.

How to get involved

We will continue to meet weekly with Traffic and Transportation Opposition Spokespeople and quarterly with all ward councillors to ensure we take into account the views of stakeholders and the local community. We will also use these meetings to provide updates on the development of schemes and to discuss potential for making any of the measures permanent.

If you have any suggestions for transport measures that can help support the city's response to the COIV-19 pandemic then please get in touch with your [ward councillor](#)¹ or log your suggestions on the [widen my path website](#)²

1. <https://democracy.portsmouth.gov.uk/mgMemberIndex.aspx?bcr=1>

2. www.widenmypath.com

Glossary of terms

Like many sectors the world of 'Transport' has its own jargon and acronyms. This glossary provides definitions for terms that are used within this document.

Accessible - This means that transport services are easy to access for all, including anybody who experiences limited mobility and, or people who may be using wheelchairs

Air Quality Management Area (AQMA) - An area where national air quality objectives set by the government are not being achieved and changes are needed to reduce air pollution levels

Artificial Intelligence - Computer systems used to carry out tasks, usually requiring human intelligence

Carbon Footprint - The total greenhouse gas emissions generated by a person, household or organisation, usually measured over the course of a year

Car Club - Offers members access to locally parked vehicles without being tied to ownership

Clean Air Zone (CAZ) - A zone where a package of measures will be brought forward to improve air quality in order to meet the national air quality objectives set by the government. It may or may not include a charging element. Portsmouth City Council has been directed by government to implement a Class B Charging CAZ

Connected Vehicles - Vehicles that use technology to communicate with each other and the world around them, to help the driver or vehicle make assisted decisions. A basic example is GPS information that helps find the quickest route

Autonomous Vehicles - Autonomous vehicles currently on the road are semi-autonomous with some functionality that does not need a driver, such as self-parking or auto-collision avoidance features. In the future there could be fully autonomous vehicles that don't need a driver at all

Collaborative Traffic Management - Use of online/cloud based traffic management systems to coordinate management of Strategic Road Network (SRN) with Local Highway Authority (LHA) networks

Consolidation centres - This is where goods are delivered from many suppliers. Then, when needed, multiple goods are collected as part of a fuller load by smaller more environmentally friendly vehicles, for example into the city centre reducing the impact of freight journeys. The centres can be:

- Macro - large centres on the edge of cities, goods are delivered by Heavy Goods Vehicles (HGVs) or trains and collected by smaller vehicles
- Micro - small centres in neighbourhoods, goods are delivered by larger vans and collected by smaller electric vans or electric cargo bikes

Council Priorities - Each year, the council sets out its priorities. These are the areas where we will be focusing our efforts to improve life in the city for everyone.

Our five priorities are:

1. Make Portsmouth a city that works together, enabling communities to thrive and people to live healthy, safe and independent lives.
2. Encourage regeneration built around our city's thriving culture, making Portsmouth a great place to live, work and visit.
3. Make our city cleaner, safer and greener.
4. Make Portsmouth a great place to live, learn and play, so our children and young people are safe, healthy and positive about their futures.
5. Make sure our Council is a caring, competent and collaborative organisation that puts people at the heart of everything we do.

Cycle Hangars - These are covered structures that provide secure cycle parking in locations where it is difficult to store bicycles. For example in residential areas where terrace properties have no rear access. In these locations the hangars are located where car parking spaces would have been, providing space for six bicycles

Demand Responsive Transport - This is a form of shared transport that can be pre-booked by individuals which offers services that have the flexibility to alter their routes based on demand for trips, rather than by using a fixed route and timetable

Electric Vehicle (EV) - A vehicle that uses one or more electric motors for propulsion. Distinct from a hybrid vehicle that uses two or more sources of power, such as diesel and electricity

Filtered Permeability - A road that allows through-access for walking and cycling, but removes it for motor traffic

First and Last Mile trips - These are the trips made at either end of a longer public transport journey and may be from your house to a transport interchange, or between a transport interchange and your place of work, a school or a leisure location. They are usually short distance trips that may not be well-served by local public transport, but may be possible to make with more flexible 'Shared Transport' or 'Demand Responsive Transport' (see individual definitions in glossary)

Future Transport Zone (FTZ) - A zone that will provide a real-world testing ground for innovative ways to transport people and goods. We are part of the Solent Transport partnership that has secured funding from government for a FTZ in Portsmouth. This will trial innovative measures to provide new modes of travel and ways to plan and pay for it, alongside innovative measures to reduce the impacts of freight movements in the city

Heavy Goods Vehicle (HGV) or Large Goods Vehicle (LGV) - A large vehicle that's purpose is to transport heavy loads (these terms cover all commercial trucks that feature a gross combination mass of over 3500kg)

Kerbside space - The area in which the carriageway (road) joins the footway (path). Kerbside space can be utilised (drop off and loading) and enforced (double yellow lines) in a manner of ways to enhance road space for users

Lane Permit Scheme - A means of requesting space to carry out work on the highway either as a statutory undertaker or as a highway authority in line with New Roads and Streetworks Act (NRSWA) 1991

Lane Rental Scheme - Allows a local highway authority to charge works promoters for the that street and road works occupy the highway

Local Cycle and Walking Infrastructure Plan (LCWIP) - A strategy document that sets out the approach to developing local cycling and walking networks over a ten-year period. This is part of the Government’s strategy to double the number of cycling journeys made and substantially increase walking activity by 2025

Local and District Centres - Focus points that encompass a grouping of units (such as shopping facilities)

Low Traffic Neighbourhood - A street or group of streets in which through vehicle traffic is removed or discouraged

Local Transport Plan (LTP) - A strategy document that sets out the vision, objectives, policies and implementation plan for improving all of the transport network. This document is the LTP, Edition 4 for Portsmouth

Micro mobility - Any range of modes making use of small vehicles, principally e-scooters and bicycles, including e-bikes

Mobility as a Service (MaaS) - An on-line platform or smartphone app that allows a user to view travel options, timing, costs associated with a range of shared and public transport modes and to book and pay for such journeys.

Mobility Hubs - Locations, usually at key transport interchanges such as train or bus stations, piers, busier bus or rapid transit stops, which provide access to a range of transport modes and facilities. These can include rail, bus, rapid transit, cycle and scooter hire, car club, cycle parking but also facilities such as shopping lockers, cafes and bike shops. They are designed to make it easier for people to access the core public transport network and make ‘first or last mile’ trips by other modes.

Mode of Transport - This refers to the different ways people and goods can be transported such as public transport, Heavy Goods Vehicles (HGVs), bicycles, walking and private car.

Mode Share - The proportion of trips made by each mode of transport.

Mode Shift - A change in the proportion of trips made by different modes of transport, often reflecting changes to the transport network or the services that use it

My Journey - Online site for travel information and advice in Hampshire

Multi-Purpose Vehicle (MPV) - A type of car that offers a more spacious interior and taller ride height than a standard car. Often referred to as a minivan or people carrier

On-Demand Buses - As with Demand Responsive Transport, these buses would operate on a pre-booked basis by individuals, rather than on a fixed route and schedule

Parklet - A green space created to be publically accessible, usually in an urbanised environment in a former roadside parking space

Play Streets - Street’s where the road is closed to through traffic for periods of time to allow children to safely play outside

Pro-Active Network Management - Use of technology and collection of data to predict changes in highway flow/use and to react to mitigate impacts of change

Public Realm - The space between buildings that is open to the public, including streets, squares, forecourts, parks and open spaces

Rapid Transit - High capacity, high frequency, road based public transport services that often run in dedicated lanes separate from general traffic with priority at junctions to ensure fast and reliable journey times. The proposed rapid transit network in Portsmouth will form part of the South East Hampshire Rapid Transit scheme

Residents Parking Zone - A zone where car parking is only permitted on-street for vehicles that have an eligible parking permit

Shared transport - This is a demand-driven vehicle-sharing arrangement, in which people share a vehicle over time, like car clubs, car share, and bike or scooter hire schemes. This can save the user money and reduces vehicles on the road

Smart Parking App - An app that allows drivers to see the availability of on street pay and display parking spaces near to their destination

Solent Go - is a pay as you go top up card that allows passengers to travel seamlessly across South Hampshire using buses and ferries

Sustainable Transport - Any form of transport that produces low or zero levels of carbon emissions, including walking, cycling and public transport

Transforming Cities Fund (TCF) - This is a funding package from government that aims to improve productivity and spread prosperity through investment in public and sustainable transport in England's city regions. Portsmouth City Council is part of a partnership that has secured funding from government from the Tranche 1 TCF, and are submitting a rebid for Tranche 2 TCF funding to bring forward the next phase of the South East Hampshire Rapid Transit network in Portsmouth

Workplace Parking Levy (WPL) - A scheme that places a charge on employers who provide workplace parking for employees. The funds raised are used towards future sustainable transport schemes

Zero Emission Vehicles - These are vehicles that produce no carbon emissions at their point of use. They may be powered by electric or other fuel sources such as hydrogen



You can get this information in large print, Braille, audio or in another language by calling 023 9284 1193

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